

MINOR RESIDENTIAL STREETS ATTACHED SIDEWALK

NOTES:

1. PAVEMENT SECTION TO BE HMA ON AB, SEE ST-20 OF THESE STANDARDS FOR STREET PAVEMENT REQUIREMENTS.
2. CURB AND GUTTER TO BE ROLLED CURB AND GUTTER (ST-06) ADJACENT TO SINGLE FAMILY RESIDENTIAL LOTS AND WITH APPROVAL FROM CITY ENGINEER. VERTICAL CURB & GUTTER (ST-08) AT ALL OTHER LOCATIONS.
3. SIDEWALK TO HAVE 4" CONCRETE ON 4" AGGREGATE BASE, SEE ST-08.
4. PRIVATE STREET SECTION'S WIDTH CAN BE REDUCED BY 5' BY ELIMINATING SIDEWALK FROM ONE SIDE OF THE STREET WITH THE APPROVAL OF CITY ENGINEER.
5. PUE/TCE - PUBLIC UTILITY EASEMENT

NO.	REVISION DATE	BY
DRAWN BY: J. GINNEVER		
CHECKED BY: D. HARDEN		
SCALE: NONE		
DATE: 2/2/26		

MINOR RESIDENTIAL STREETS ATTACHED SIDEWALK

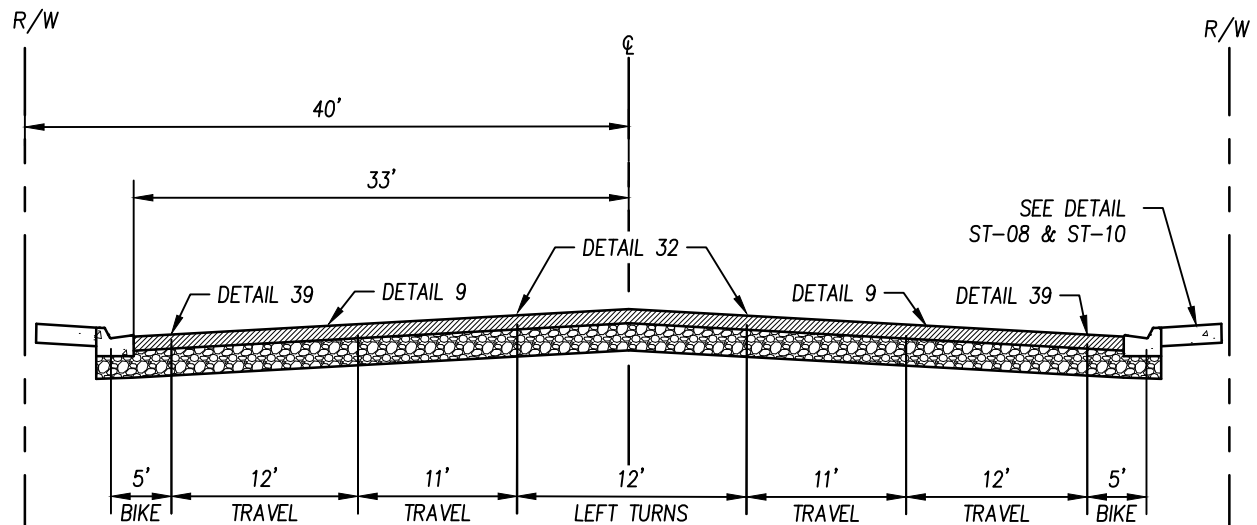
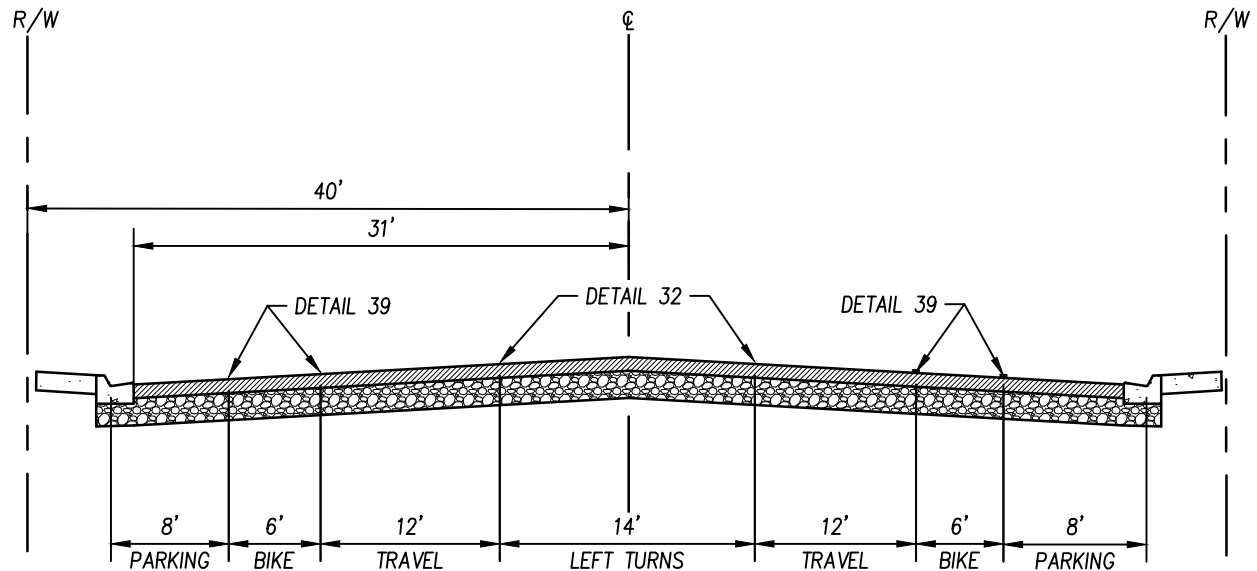
CITY OF GRIDLEY
DEPARTMENT OF PUBLIC WORKS

APPROVED BY:
DAVID HARDEN

David Harden
CITY ENGINEER
RCE 84216



ST-01



NOTES:

1. MINOR ARTERIAL SHALL BE USED WHEN PARKING IS ALLOWED TO BE INSTALLED. STRIPING AND SIGNS SHALL BE IN ACCORDANCE WITH THE MOST CURRENT EDITION OF THE CALIFORNIA MUTCD.
2. ARTERIAL SHALL BE USED WHEN NO PARKING IS ALLOWED TO BE INSTALLED.
3. PAVEMENT SECTION SHALL BE HMA ON AB SEE ST-20.
4. CURB AND GUTTER SHALL BE VERTICAL CURB & GUTTER (ST-06 AND ST-08).
4. PUBLIC UTILITY EASEMENT (PUE) SHALL EXTEND 10' FROM RIGHT OF WAY (R/W) INTO ADJACENT PROPERTY

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ARTERIAL STREETS

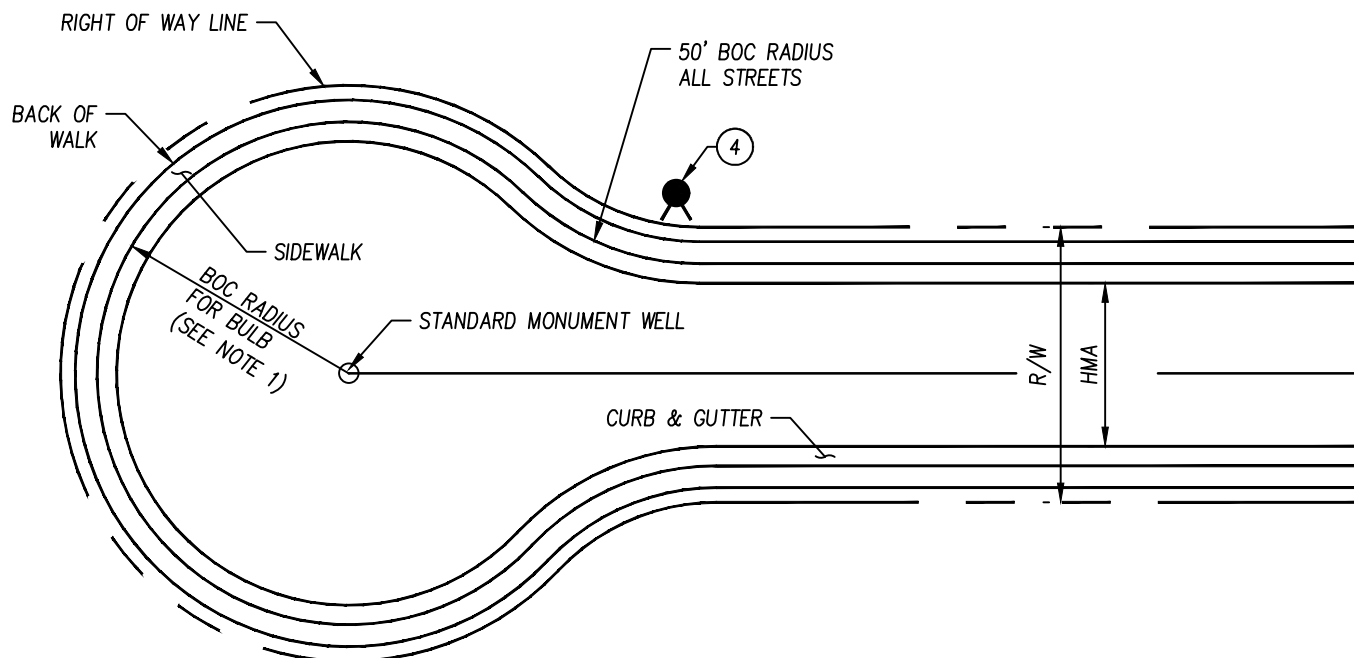
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CITY ENGINEER
RCE 84216



ST-03



WIDTHS OF SURFACING, SHOULDERS AND
RIGHT OF WAY OF CONNECTING ROAD VARIES

NOTES:

1. FACE OF CURB RADIUS FOR BULB SHALL BE AS FOLLOWS:
INDUSTRIAL/RESIDENTIAL STREET - 30'
2. LENGTH OF CUL-DE-SAC STREETS SHALL BE MEASURED FROM CENTER OF BULB TO CENTER LINE OF INTERSECTING STREET. CUL-DE-SAC LENGTH SHALL NOT EXCEED 500' WITHOUT THE APPROVAL OF THE CITY ENGINEER.
3. ALL CUL-DE-SACS IN EXCESS OF 200' SHALL HAVE A FIRE HYDRANT INSTALLED AT THE BULB.

NO.	REVISION DATE	BY

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CUL-DE-SAC STREET

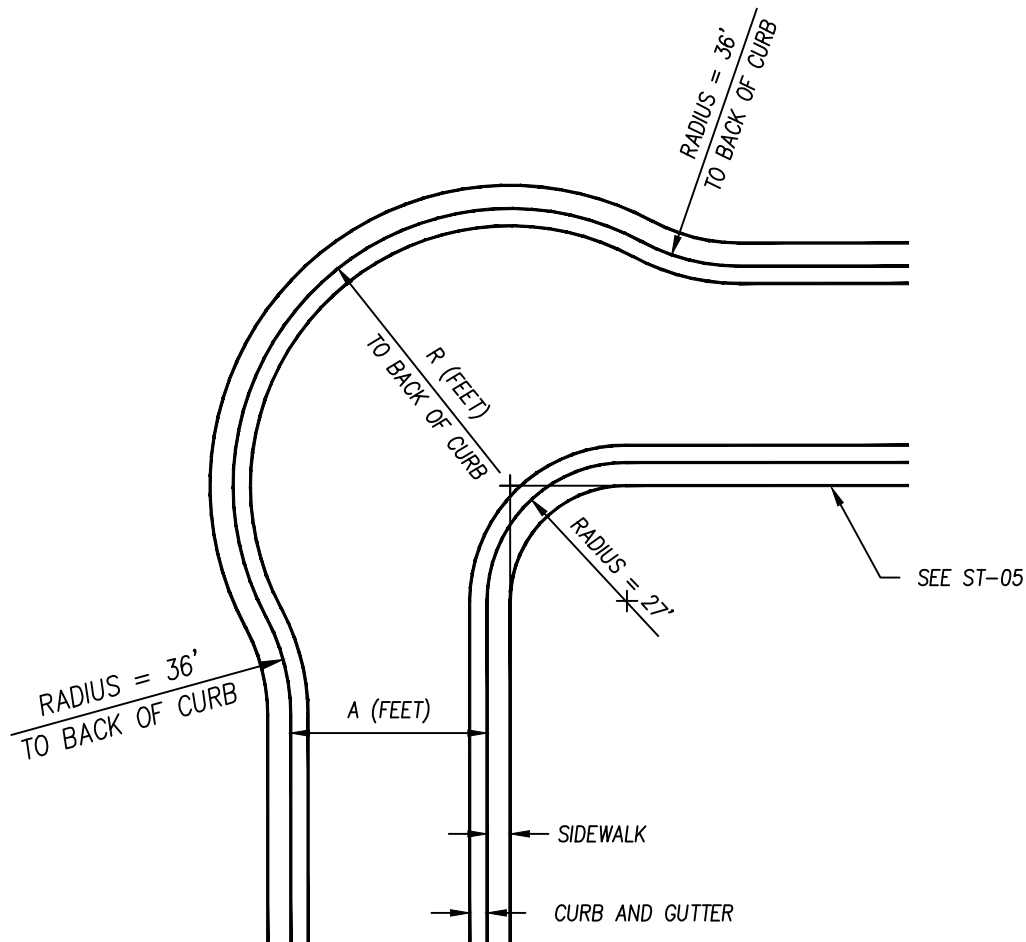
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 DAVID HARDEN

[Signature]
 CITY ENGINEER
 RCE 84216



ST-04



STREET TYPE
INDUSTRIAL

R (FEET)
60

A (FEET)

34
38
40
48

R (FEET)

48
52
54
62

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DATE: 2/2/26

STANDARD ELBOW

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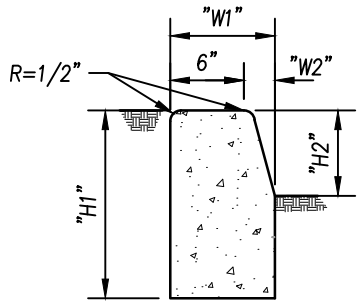
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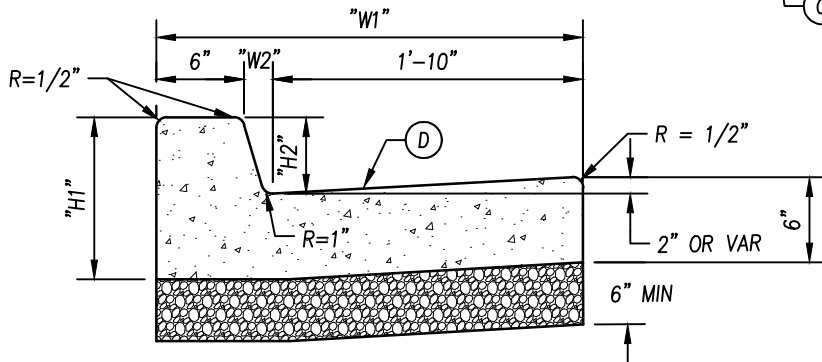


ST-05

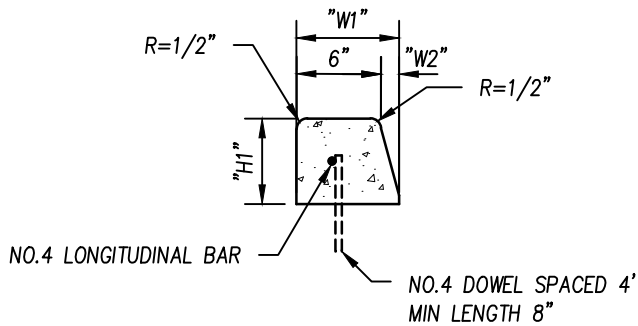
VERTICAL CURBS TYPES A1, A2, A3



TYPE A1 CURBS
SEE TABLE A



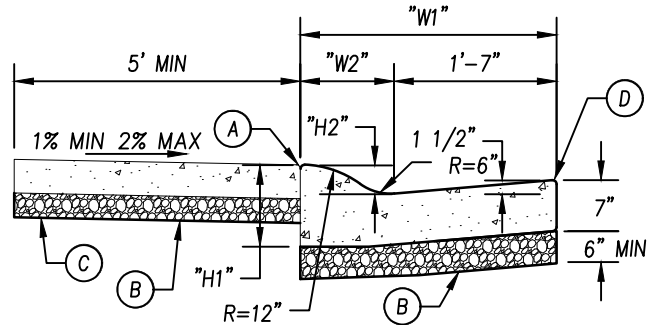
TYPE A2 CURBS
SEE TABLE A



TYPE A3 CURBS
SUPER IMPOSED ON EXISTING PAVEMENT
SEE TABLE A

TABLE A

CURB TYPE	DIMENSIONS			
	"H1"	"H2"	"W1"	"W2"
A1-6	1'-2"	6"	7 1/2"	1 1/2"
A2-6	1'-0"	6"	2'-6"	2"
A2-8	1'-2"	8"	2'-6"	2"
A3-6	6"		7 1/4"	1 1/4"
A3-8	8"		7 3/4"	1 3/4"
A4	9 5/8"	3 3/8"	2'-6"	11"



A4 ROLLED CURB & GUTTER

LEGEND:

- A. 1/8" DEEP SCORE MARK OR COLD JOINT
- B. CLASS 2 AGGREGATE BASE PROCESSED TO 95% RELATIVE COMPACTION.
- C. ALL SIDEWALK SHALL BE 4" CONCRETE ON 4" AGGREGATE BASE, SEE DETAIL ST-08
- D. GUTTER PAN CROSS SLOPE NOT TO EXCEED 5% MAX WHEN ADJACENT TO CURB RAMP, OR BE LESS THAN 4%.

NOTES:

- 1. MINIMUM WIDTH OF CLEAR PASSAGEWAY FOR SIDEWALK SHALL BE 5'.
- 2. RETAINING CURBS AND ACQUISITION OF CONSTRUCTION EASEMENT MAY BE NECESSARY FOR NARROW SIDEWALKS OR CURB HEIGHTS IN EXCESS OF 6"
- 3. ALL ADJOINING SIDEWALK, CURB AND GUTTER MAY BE POURED MONOLITHICALLY.

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CURB AND GUTTER

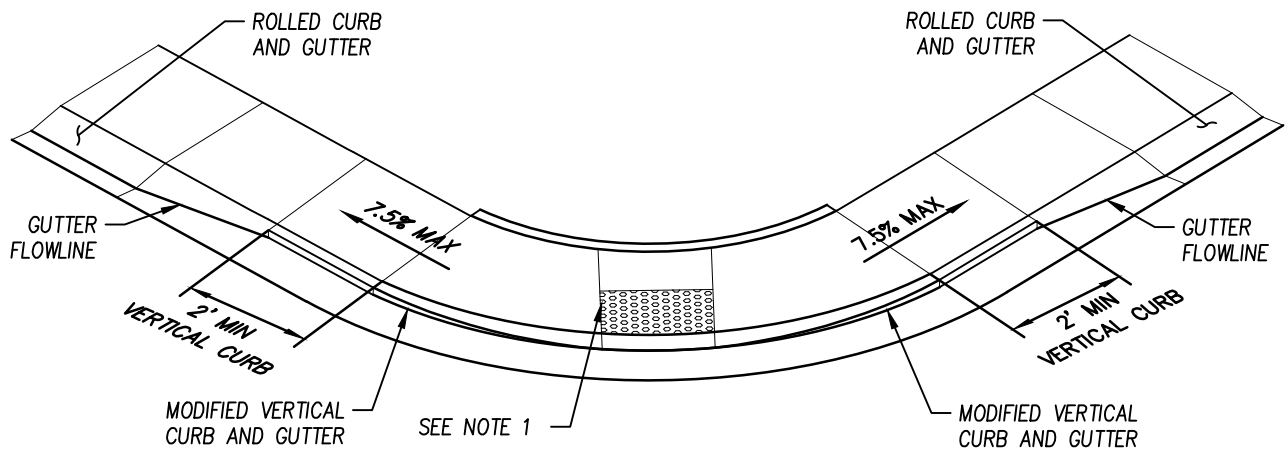
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CITY ENGINEER
RCE 84216



ST-06



NOTE:

1. ALL CURB RAMPS SHALL CONFORM TO THE MOST CURRENT CALTRANS STANDARDS AND ACCESSIBILITY STANDARDS.
2. SIDEWALK TRANSITION RUNNING SLOPE NOT TO EXCEED 5% OR ADJACENT ROADWAY GRADE, WHICHEVER IS GREATER.

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CURB AND GUTTER TRANSITION

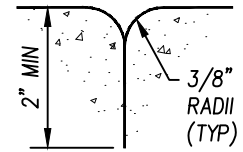
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ST-07



ST-08

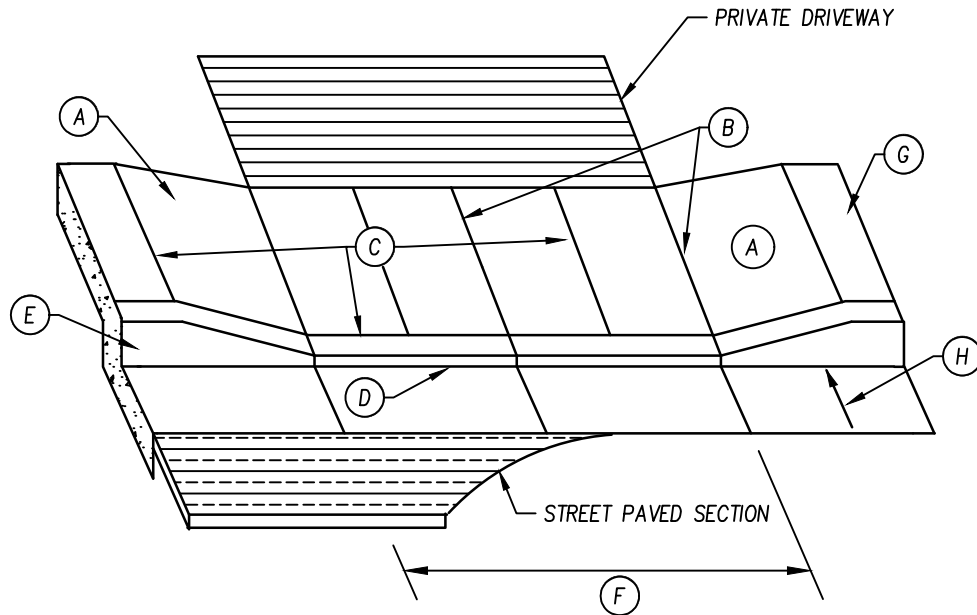


1. ALL WORK TO BE DONE, AND ALL MATERIAL SUPPLIED, SHALL CONFORM TO THE CALTRANS STANDARD SPECIFICATIONS.
2. STEEL REINFORCEMENT SHALL CONFORM TO THE CALTRANS STANDARD SPECIFICATIONS.
3. BROOM FINISH SHALL BE PARALLEL TO THE DIRECTION OF FLOW.
4. FLOW LINE ELEVATIONS MUST BE APPROVED BY CITY ENGINEER.

NO.	REVISION DATE	BY	<div style="text-align: center;"> <h1>VALLEY GUTTER</h1> </div>	<div> <div>APPROVED BY: DAVID HARDEN</div> <div>  </div> <div>CITY ENGINEER RCE 84216</div> </div> <div>  </div>
DRAWN BY: J. GINNEVER				
CHECKED BY: D. HARDEN			<div style="text-align: center;"> <h2>CITY OF GRIDLEY</h2> <h3>DEPARTMENT OF PUBLIC WORKS</h3> </div>	<div style="text-align: center;"> <h1>ST-09</h1> </div>
SCALE: NONE				
DATE: 2/2/26				

LEGEND:

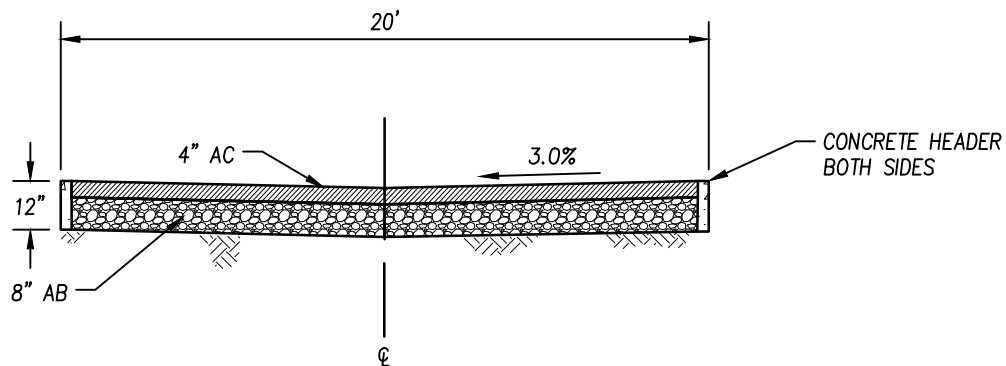
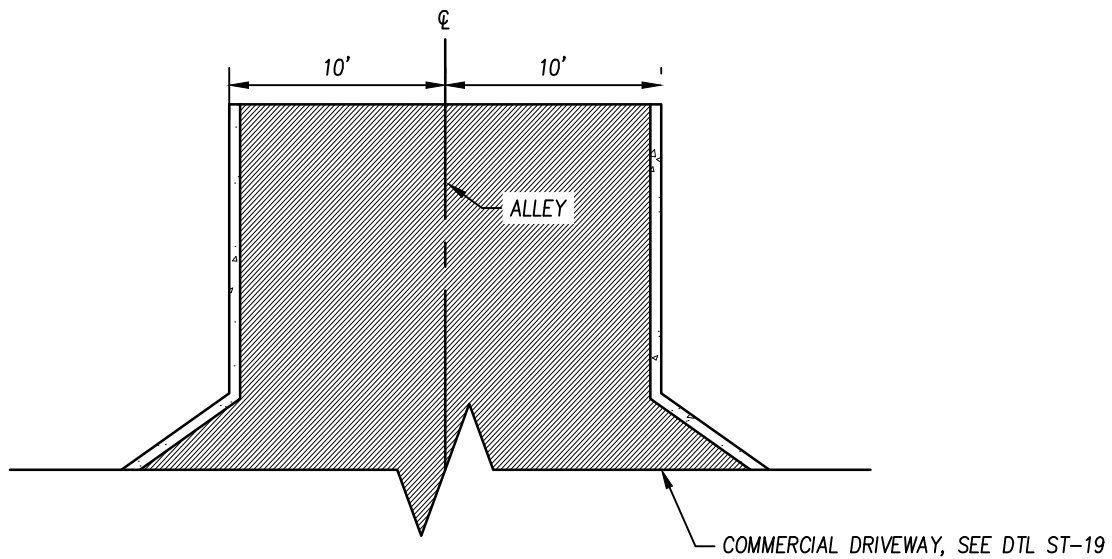
- A. SLOPE SHALL NOT EXCEED 7.5% FOR 6" CURB AND 5% FOR 8" CURB. MAX LENGTH SHOULD BE 15'.
- B. "TOOL JOINT" SEE DETAIL ST-08.
- C. SCORE MARKS EVERY 5' FOR SIDEWALK
- D. 1" HIGH LIP AT 45° BATTER.
- E. "A2"(ST-06) CURB AND GUTTER.
- F. DRIVEWAY WIDTH PER THE APPROVED PLAN. (MIN 24' AND MAX 40' FOR COMMERCIAL, MIN 12' AND MAX 24' FOR RESIDENTIAL)
- G. ADJACENT SIDEWALK.
- H. GUTTER PAN CROSS SLOPE NOT TO EXCEED 5% MAX.



NOTES:

1. SIDEWALK CROSS GRADE THROUGH THE ENTIRE DRIVEWAY SHALL BE 1% MIN, 2% MAX.
2. FOR COMMERCIAL DRIVEWAY: SIDEWALK, APRON AND GUTTER PAN SHALL BE 6" THICK WITH NO.4, GRADE 60 REBAR ON 18" CENTER EACH WAY. USE 3" DOBIES (REBAR SPACERS) AT 3' INTERVALS. FOR RESIDENTIAL: WALK, APRON, AND GUTTER PAN SHALL BE 6" THICK NONREINFORCED. FOR COMMERCIAL AND RESIDENTIAL, SIDEWALK, APRON AND GUTTER PAN SHALL BE PLACED MONOLITHICALLY. ALL CONCRETE SHALL BE "MINOR CONCRETE" AS DEFINED IN CALTRANS STANDARD SPECIFICATIONS.
3. BASE FOR CONCRETE SHALL BE CLASS 2, 3/4" AGGREGATE BASE, EITHER PROCESSED 6" THICK TO 95% RELATIVE COMPACTION.
4. AC PATCH ADJACENT TO GUTTER LIP SHALL CONFORM TO "ASPHALT CONCRETE" SECTION OF CALTRANS STANDARD SPECIFICATIONS.
5. FROM 8' TO 20' BACK OF SIDEWALK, RAISE PRIVATE PAVING/LANDSCAPING TO A MINIMUM HEIGHT OF THE HIGHEST TOP OF CURB ELEVATION AT THE DRIVEWAY.

NO.	REVISION DATE	BY	<h2 style="margin: 0;">DRIVEWAY APRON WITH ATTACHED SIDEWALK</h2>	APPROVED BY: DAVID HARDEN	
				 CITY ENGINEER RCE 84216	
DRAWN BY: J. GINNEVER					
CHECKED BY: D. HARDEN					
SCALE: NONE			CITY OF GRIDLEY DEPARTMENT OF PUBLIC WORKS	<h1 style="margin: 0;">ST-10</h1>	
DATE: 2/2/26					



NOTES:

1. ALLEY ENTRANCE SHALL BE DESIGNED AS A COMMERCIAL DRIVEWAY IN TERMS OF REBAR
2. TRANSITION FROM DRIVEWAY TO ALLEY SHALL BE MADE BETWEEN BACK OF WALK AND RIGHT OF WAY. VARIATION FROM THIS MAY REQUIRE DEDICATION OF ADDITIONAL RIGHT OF WAY FOR TRANSITION.
3. ALL CONCRETE TO BE "MINOR CONCRETE" AS DEFINED IN CALTRANS STANDARD SPECIFICATIONS.



NO.	REVISION DATE	BY	ALLEY DETAIL	APPROVED BY: DAVID HARDEN	
DRAWN BY: J. GINNEVER					
CHECKED BY: D. HARDEN			CITY OF GRIDLEY DEPARTMENT OF PUBLIC WORKS	CITY ENGINEER RCE 84216	
SCALE: NONE					
DATE: 2/2/26					
			ST-11		

TABLE 1

TRAFFIC INDEX FOR RESIDENTIAL STREETS	
NUMBER OF RESIDENTIAL UNITS SERVED	TRAFFIC INDEX
0-24	4.5
25-40	5.0
41-90	5.5
91-180	6.0
181-300	6.5
301-500	7.0
501-700	7.5
701-900	8.0



TABLE 2

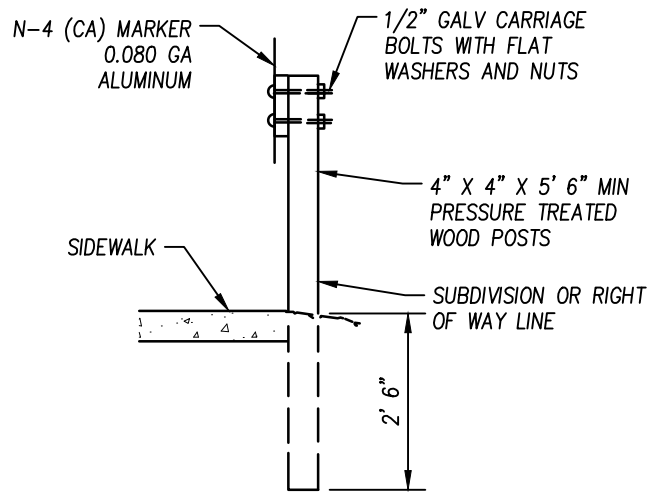
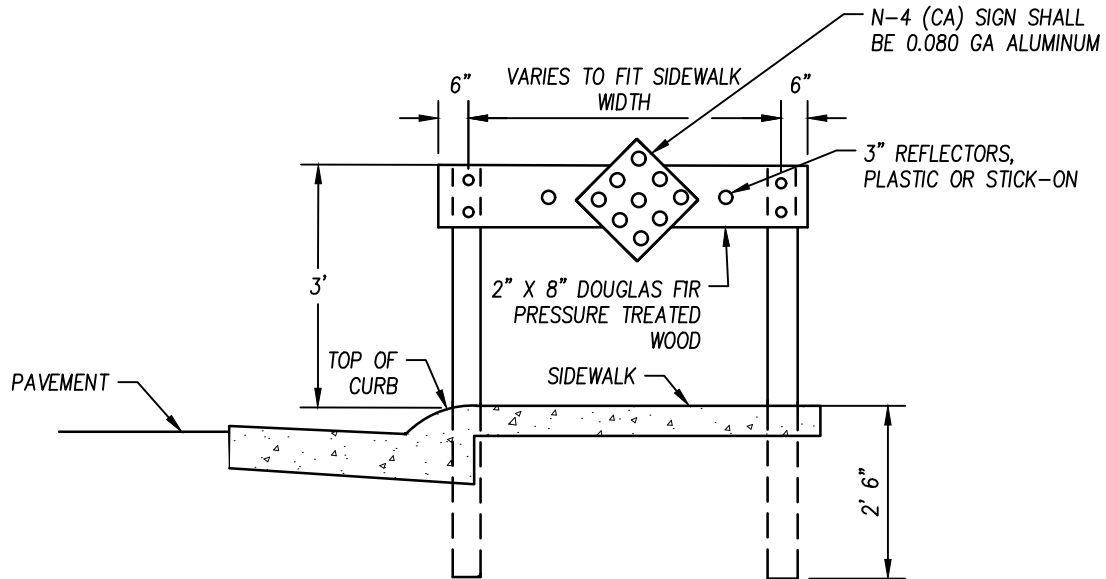
TRAFFIC INDEX BY STREET CLASS	
LOCAL COMMERCIAL AND MINOR COLLECTOR	7.0
LOCAL INDUSTRIAL AND MAJOR COLLECTOR	8.0
ARTERIAL	9.0

TABLE 3

STRUCTURAL PAVEMENT SECTIONS (INCHES OF AC OVER AB)					
T.I.	R=5	R=10	R=15	R=20	12" LTB
4.5	4/8	4/8	4/8	4/8	4/6
5.0	4/8	4/8	4/8	4/8	4/6
5.5	4/9	4/8	4/8	4/8	4/6
6.0	4/12.5	4/11	4/9.5	4/9.5	4/6
6.5	4/14	4/12.5	4/11.5	4/11.5	4/8
7.0	4/16	4/14.5	4/13.5	4/13.5	4/8
7.5	5/16	5/14.5	5/13.5	5/13.5	5/9
8.0	5/17.5	5/16	5/15	5/15	5/10
8.5	5/20	5/18	5/16.5	5/16.5	5/12
9.0	6/20	6/18	6/16.5	6/16.5	6/12
9.5	6/22	6/20	6/18.5	6/18.5	6/16
10.0	8/22	8/20	6/20	6/20	6/16

1. EVALUATING WHETHER A STREET SEGMENT SERVES A GIVEN RESIDENTIAL UNIT IS BASED ON VEHICLES FROM THE RESIDENCE BEING LIKELY TO UTILIZE THE SEGMENT DURING THE MORNING OR EVENING PEAK TRAFFIC HOURS, AS DETERMINED BY A REASONABLE TRAFFIC DISTRIBUTION ASSUMED FOR THE AREA.
2. IN THE ABSENCE OF TRAFFIC PROJECTIONS FOR MIXED-USE OR NON-RESIDENTIAL AREAS, TABLE 1 MAY BE USED AS A GUIDE FOR DETERMINING THE TRAFFIC INDEX.
3. TABLE 2 IS TO BE USED ONLY WHEN THERE IS NO OTHER INFORMATION AVAILABLE UPON WHICH TO BASE A TRAFFIC INDEX CALCULATION. THE TRAFFIC INDICES SHOWN ARE APPROPRIATE WHERE HIGH VOLUMES OF HEAVY VEHICLES ARE EXPECTED. THE CITY SHALL MAKE THE FINAL DETERMINATION AS TO THE APPROPRIATENESS OF THE ASSUMPTIONS AND METHODOLOGY USED FOR TRAFFIC INDEX DETERMINATION.
4. IN CERTAIN CASES, SUB-BASE STABILIZATION BY MEANS OF LIME TREATMENT (LTB) FOR CLAY SOIL OR THE CEMENT TREATMENT OF BASE MATERIALS MAY BE APPROVED. WHEN APPROVED, A MINIMUM OF 12" OF SOIL OR BASE IS TO BE TREATED A MAXIMUM R-VALUE OF 30 IS TO BE APPLIED TO THE STRUCTURAL SECTION CALCULATIONS. THE MINIMUM THICKNESS OF AGGREGATE BASE TO BE USED BETWEEN THE ASPHALT AND TREATED SUB-BASE IS 6". THE CITY WILL REQUIRE SOILS SAMPLING AND TESTING OF TREATED BASES OR SUB-BASES TO ENSURE THAT ADEQUATE STABILIZATION HAS BEEN PROVIDED. LIME TREATMENT OF SUB-GRADE MAY BE REQUIRED IF CONSTRUCTION OCCURS DURING WET MONTHS OF THE YEAR.
5. THE MINIMUM STRUCTURAL SECTION IS 4" OF ASPHALT OVER 8" OF AGGREGATE BASE. ALLEYS PROVIDING ACCESS TO FIRE STATIONS ARE TO HAVE STRUCTURAL SECTIONS WITH A MINIMUM OF 8" PCC AND OVER 8" AB AND WILL REQUIRE REINFORCING STEEL (NO.4 BARS @ 24"OC, TYP).
6. THE INFORMATION PROVIDED IN TABLE 3 IS BASED ON THE CALTRANS HIGHWAY DESIGN MANUAL. THE DESIGN ENGINEER MAY REFERENCE THIS MANUAL TO VERIFY THAT THE STRUCTURAL SECTION IS ADEQUATE. STRUCTURAL SECTIONS MAY BE DETERMINED BY INTERPOLATION OF THE DATA PROVIDED IN THE TABLE.

NO.	REVISION DATE	BY	<h2 style="text-align: center;">MINIMUM STRUCTURAL SECTION</h2>	APPROVED BY: DAVID HARDEN  CITY ENGINEER RCE 84216	
DRAWN BY: J. GINNEVER			<h2 style="text-align: center;">CITY OF GRIDLEY</h2> DEPARTMENT OF PUBLIC WORKS	<h1 style="text-align: center;">ST-12</h1>	
CHECKED BY: D. HARDEN					
SCALE: NONE					
DATE: 2/2/26					



NOTES:

1. SIDEWALK BARRICADES SHALL BE ERECTED WHERE SATISFACTORY PROVISIONS CANNOT BE MADE FOR PEDESTRIAN TO CONTINUE BEYOND THE TERMINUS OF SIDEWALK.
2. ALL EXPOSED WOOD SURFACES SHALL BE PAINTED WITH TWO (2) COATS OF WHITE PAINT CONFORMING TO "PAINT" SECTION OF CALTRANS STANDARD SPECIFICATIONS.

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DATE: 2/2/26		

TEMPORARY SIDEWALK BARRICADE

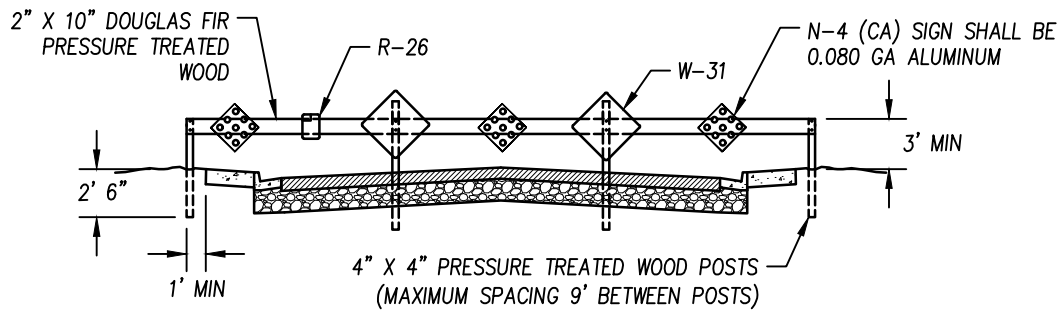
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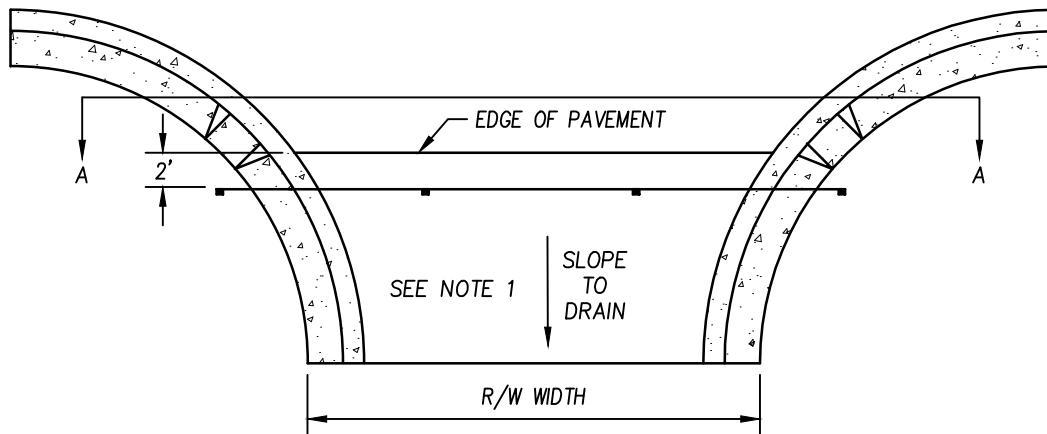
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RCE 84216



ST-13





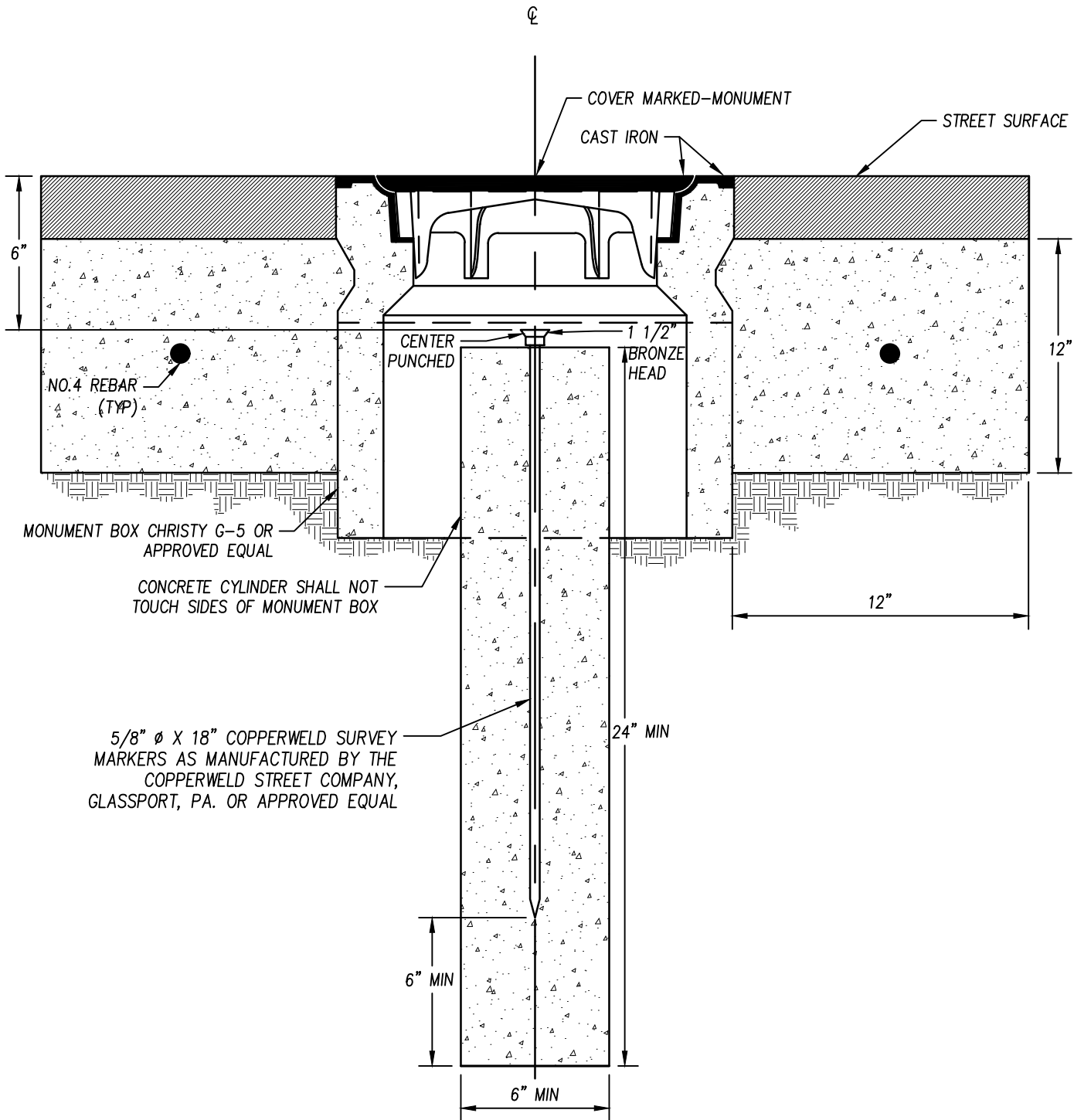
SECTION A-A



NOTES:

1. CLASS 2 AGGREGATE BASE MATERIAL PER ST-20
2. SIGNS SHALL BE IN ACCORDANCE WITH THE MOST CURRENT EDITION OF THE CALIFORNIA MUTCD.
2. ALL EXPOSED WOOD SURFACES SHALL BE PAINTED WITH TWO (2) COATS OF WHITE PAINT CONFORMING TO "PAINT" SECTION OF CALTRANS STANDARD SPECIFICATIONS.
3. ALL FASTENED POINTS SHALL USE 1/2" DIAMETER GALVANIZED CARRIAGE BOLTS WITH FLAT WASHERS AND NUTS. (DEFORM THREADS TYP)
4. TEMPORARY CUL-DE-SACS MAY BE APPROVED BY THE CITY ENGINEER IN LIEU OF DEAD END STREETS WHEN SUBDIVISION IS ADJOINING LANDS.

NO.	REVISION DATE	BY	STUB STREET BARRICADE	APPROVED BY: DAVID HARDEN	
DRAWN BY: J. GINNEVER			CITY OF GRIDLEY DEPARTMENT OF PUBLIC WORKS	 CITY ENGINEER RCE 84216	
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SCALE: NONE					
DATE: 2/2/26					



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SCALE: NONE

DATE: 2/2/26

BOXED SURVEY MONUMENT

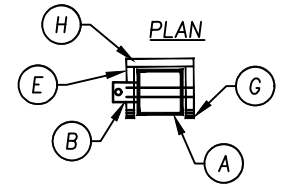
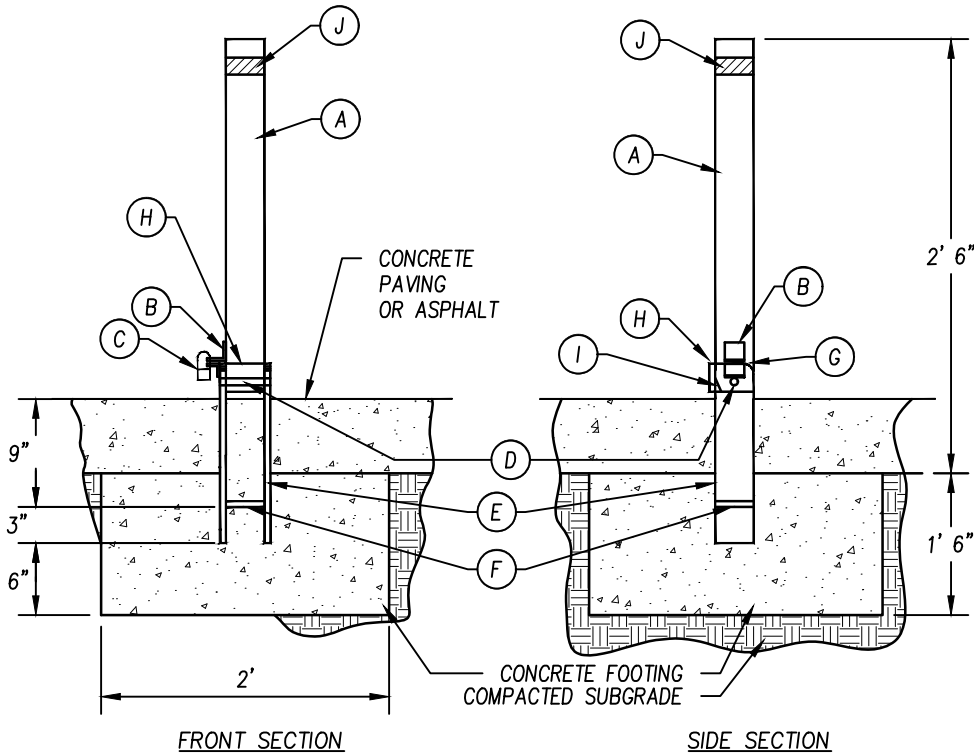
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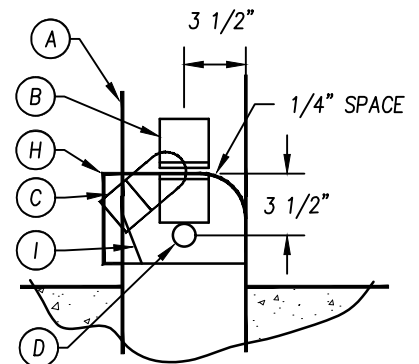
D. Harden
CITY ENGINEER
RCE 84216



ST-15



ENLARGEMENT



LEGEND:

- 3 1/2" OD 1/4" WALL STEEL TUBE WITH CAP WELDED ON TOP, 3/4" HOLES FOR SWIVEL ROD. EASE ALL EDGES OF STEEL TUBE.
- 1 1/2" X 1 1/2" ANGLE IRON (2) WELDED TO TUBE AND BASE. PLACE 1/2" HOLE DRILLED 1/4" FROM END OF ANGLE IRON. ANGLE IRON ON BASE SIZE AS NECESSARY TO EQUAL LENGTH OF ANGEL IRON ON TUBE.
- PADLOCK, TO BE PROVIDED BY CITY.
- 5/8" DIA. STEEL SWIVEL ROD, WELD SWIVEL ROD TO SIDE PLATES.
- 3/8" X 16" X 4" STEEL BASE PLATE WITH 1 1/2" RADIUS CORNERS. EASE ALL EDGES.
- 3/8" STEEL BRACE. FILET WELD BOTH SIDES TO BASE PLATES.
- 1 1/2" RADIUS CORNERS, TYP.
- 4" X 4" X 3/8" STEEL BACK PLATE WELDED TO BASE PLATE.
- FISH MOUTH GRIND AT BOTTOM BACK CORNER OF TUBE.
- PLACE 2" WHITE REFLECTIVE TAPE.

NOTES:

- ALL TUBING SHALL BE BLACK STEEL PIPE.
- ALL JOINTS SHALL BE WELDED IN ACCORDANCE WITH CA STATE STANDARD SPECIFICATIONS FOR WELDING STRUCTURAL STEEL AND GROUND SMOOTH.
- ALL PARTS (EXCEPT PADLOCK) SHALL BE PAINTED WITH TWO COATS ZINC CHROMATE PRIMER AND TWO COATS EXTERIOR ENAMEL OR FACTORY COATED. COLOR: YELLOW
- BOLLARD SHALL BE INSTALLED SUCH THAT IT LAYS FLAT WHEN FOLDED.
- CONCRETE SHALL BE "MINOR CONCRETE" AS DEFINED IN CALTRANS STANDARD SPECIFICATIONS.

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DATE: 2/2/26		

COLLAPSIBLE BOLLARD

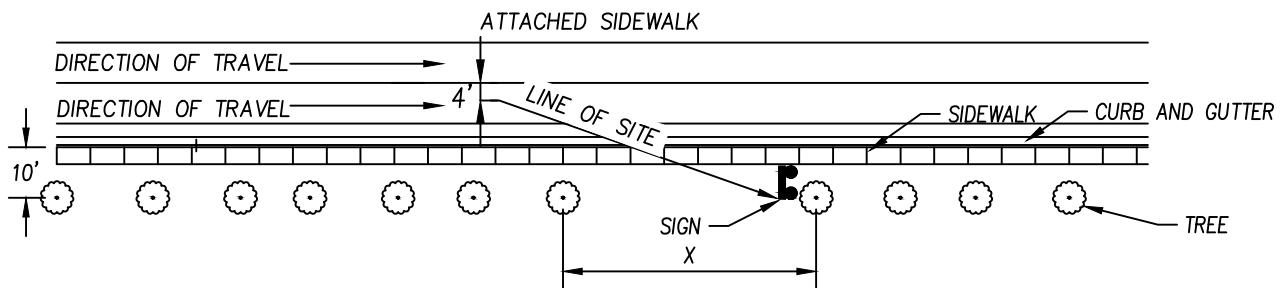
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DAVID HARDEN

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ST-16

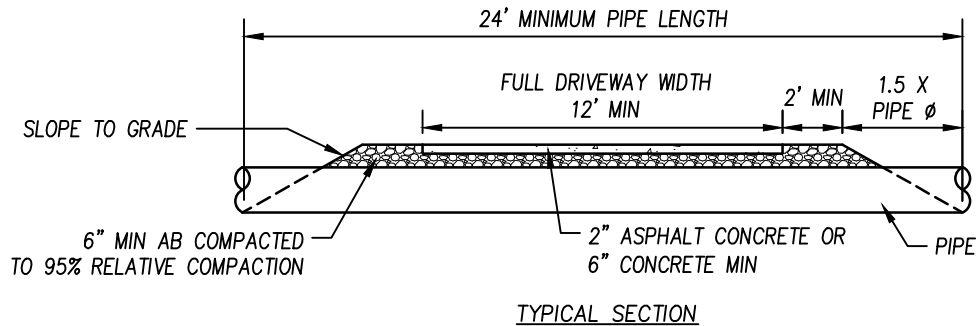
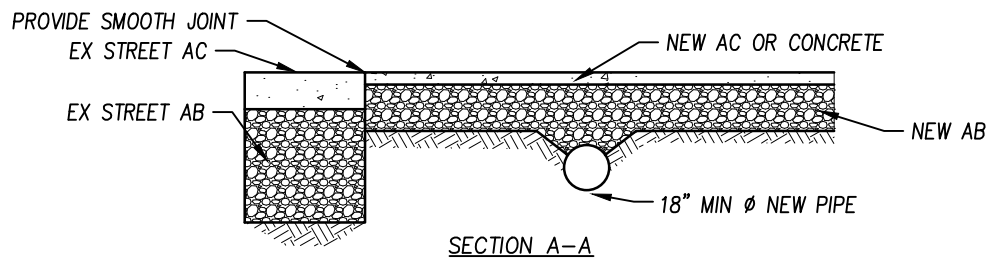
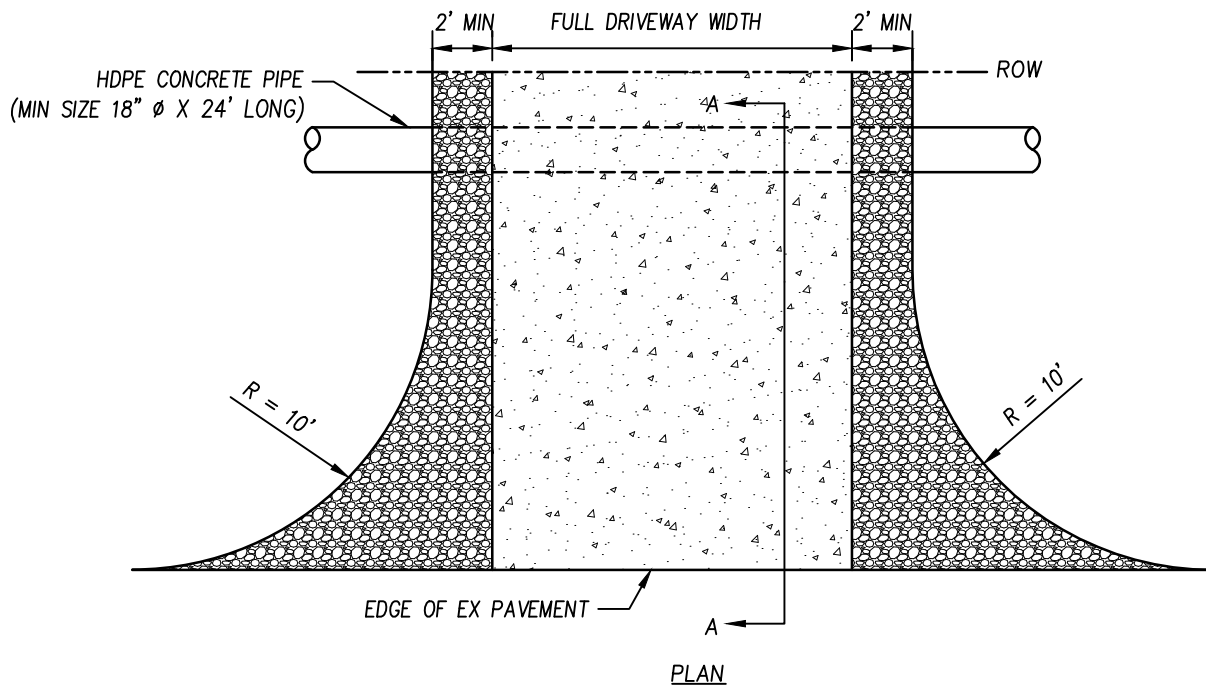


NOTES:

1. THESE ARE MINIMUM DISTANCES FOR TREES ALONG ROADWAYS.
2. DISTANCE FROM BACK OF CURB TO CENTER OF TREE IS TO BE GREATER THEN OR EQUAL TO 10'

SPEED MPH	MINIMUM CLEARANCE FOR LINE OF SITE "X"
25	75-FT
30	90-FT
40	120-FT
50	150-FT
60	180-FT

NO.	REVISION DATE	BY	LANDSCAPE SIGHT DISTANCE	APPROVED BY: DAVID HARDEN		
DRAWN BY: J. GINNEVER			CITY OF GRIDLEY DEPARTMENT OF PUBLIC WORKS	 CITY ENGINEER RCE 84216	ST-17	
CHECKED BY: D. HARDEN						
SCALE: NONE						
DATE: 2/2/26						



NOTES:

1. REFER TO ST-20, ST-21 AND DR-10 FOR BACKFILL REQUIREMENTS.
2. "MINOR CONCRETE" AS DEFINED IN CALTRANS STANDARD SPECIFICATION
3. CULVERT PIPE SHALL HAVE 12" OF MINIMUM COVER. ANY LESS REQUIRES APPROVAL FROM CITY ENGINEER.
4. CMP ALLOWED WITH APPROVAL FROM CITY ENGINEER.

NO.	REVISION DATE	BY
DRAWN BY: J. GINNEVER		
CHECKED BY: D. HARDEN		
SCALE: NONE		
DATE: 2/2/26		

STANDARD DRIVEWAY CULVERT

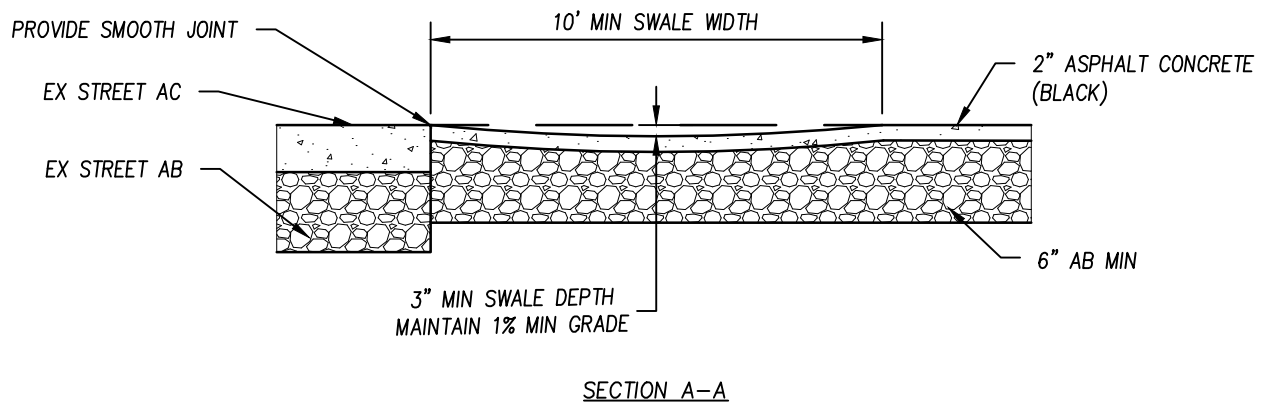
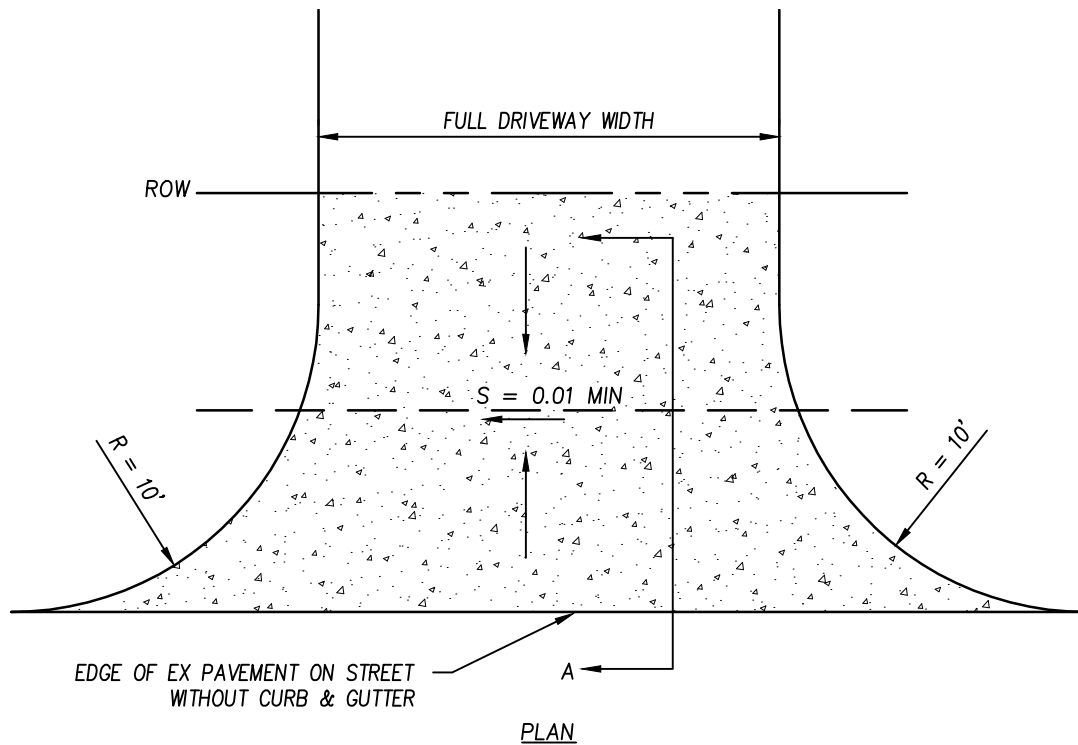
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DEPARTMENT OF PUBLIC WORKS

APPROVED BY:
DAVID HARDEN

David Harden
CITY ENGINEER
RCE 84216



ST-18



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DRIVEWAY APPROACH SWALE

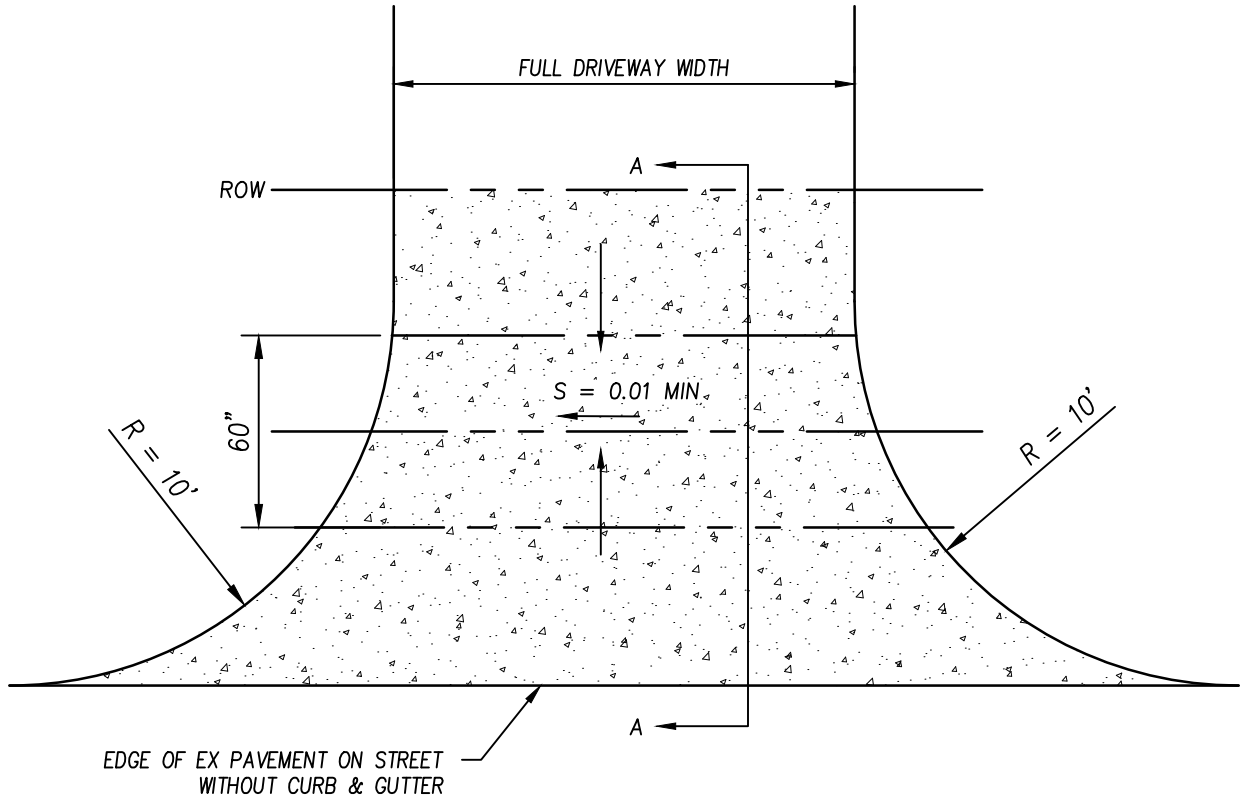
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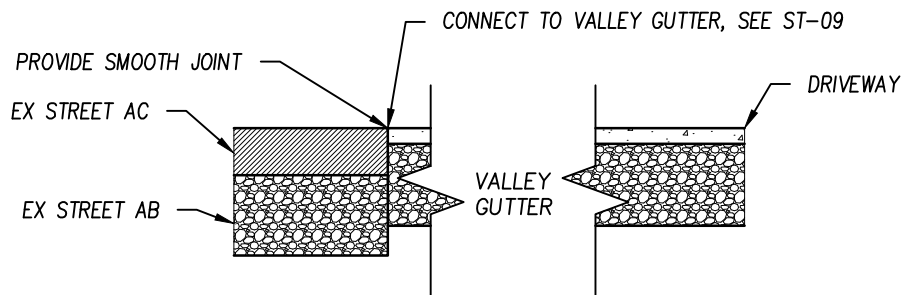
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CITY ENGINEER
RCE 84216



ST-31



PLAN



SECTION A-A

NO.	REVISION DATE	BY

DRAWN BY: J. GINNEVER

CHECKED BY: D. HARDEN

SCALE: NONE

DATE: 2/2/26

DRIVEWAY APPROACH VALLEY GUTTER

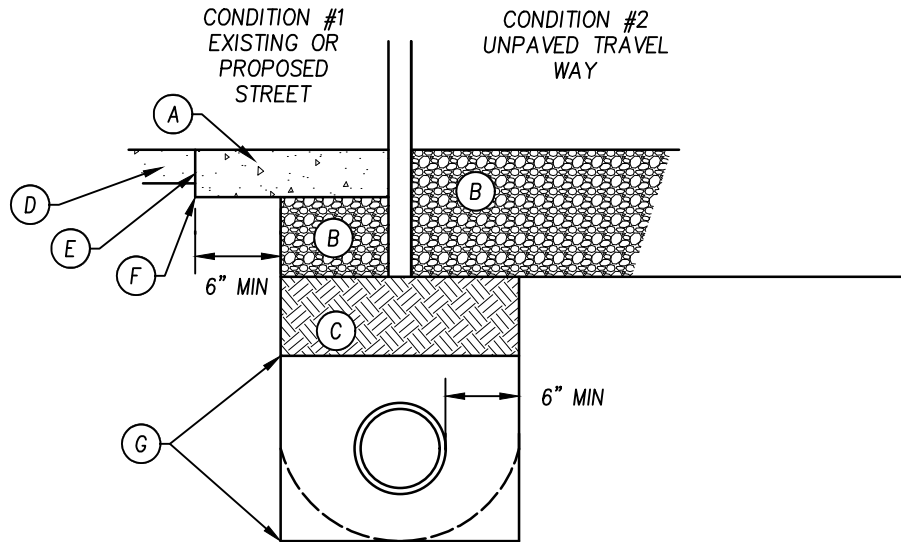
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RCE 84216



ST-19




LEGEND:

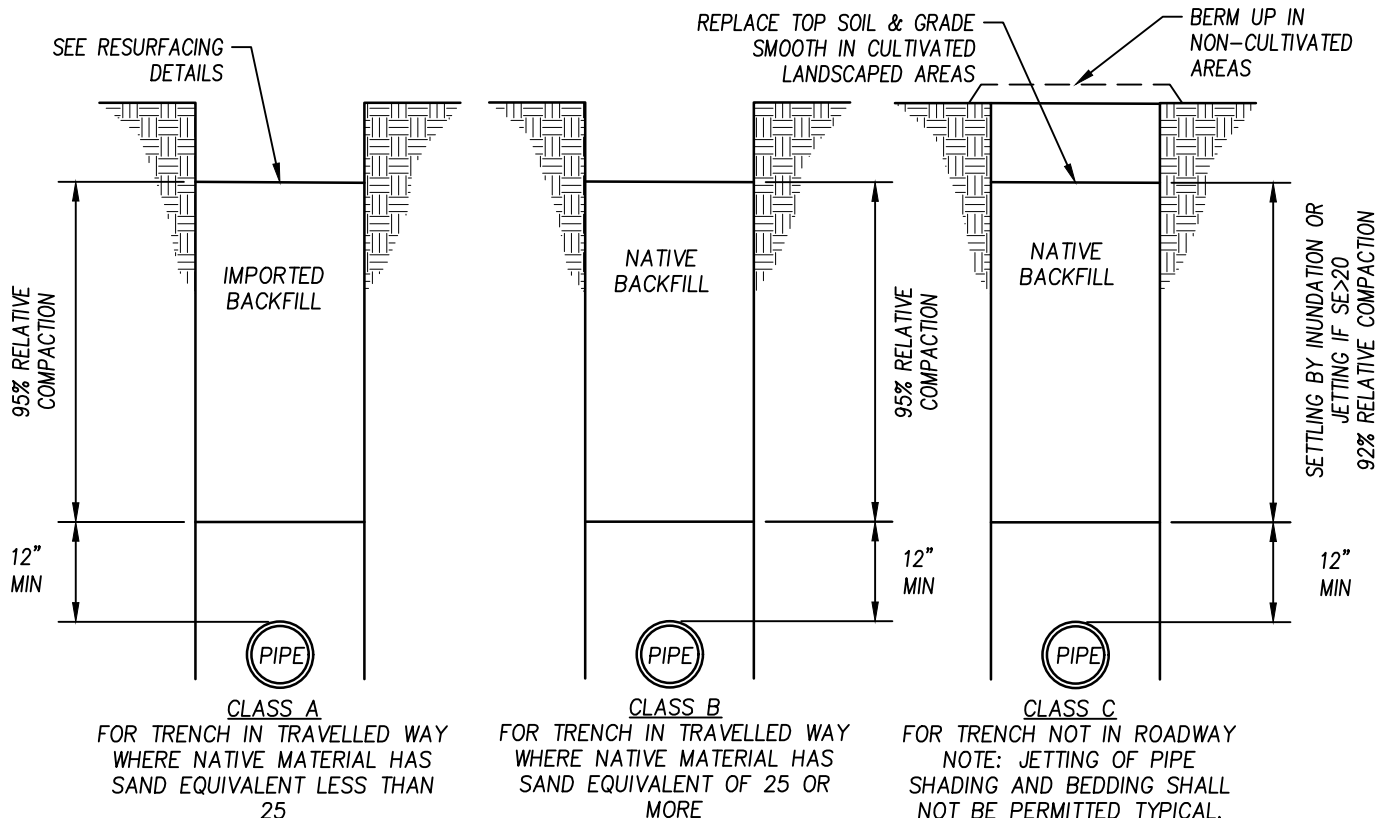
- A. ASPHALT CONCRETE PATCH OR CONCRETE OR ASPHALT CONCRETE PAVEMENT SECTION PER APPROVED PLANS:
LOCAL STREETS: 2" AC
COLLECTOR STREETS: 2" AC
COMMERCIAL AND ARTERIAL STREETS: 3" AC PLACED IN TWO LIFTS
INDUSTRIAL STREETS: 3" AC PLACED IN TWO LIFTS
- B. CLASS 2 AGGREGATE BASE:
UNPAVED TRAVEL WAY: 6" AB AT 95% RELATIVE COMPACTION
LOCAL STREETS: 6" AB AT 95% RELATIVE COMPACTION
COLLECTOR STREETS: 8" AB AT 95% RELATIVE COMPACTION
COMMERCIAL AND ARTERIAL STREETS: 8" AB AT 95% RELATIVE COMPACTION
INDUSTRIAL STREETS: 12" AB AT 95% RELATIVE COMPACTION
- C. IF MINIMUM COVER IS NOT MET OR THERE IS LESS THAN 1' BETWEEN CROSSING UTILITIES, SLURRY BACKFILL SHALL BE USED. SEE DR-10 FOR STORM DRAIN, W-15 & W-16 FOR WATER PIPE, AND SS-05 FOR SANITARY SEWER PIPE, OF THESE CONSTRUCTION STANDARDS
- D. EXISTING STREET PAVEMENT
- E. TACK COAT ON ALL VERTICAL PAVEMENT SAWCUTS
- F. TYPICAL EACH SIDE OF TRENCH IN EXISTING STREET
- G. FOR PIPE EMBEDMENT SEE DR-10 FOR STORM DRAIN, W-15 & W-16 FOR WATER PIPE, AND SS-05 FOR SANITARY SEWER PIPE, OF THESE CONSTRUCTION STANDARDS

NOTES:

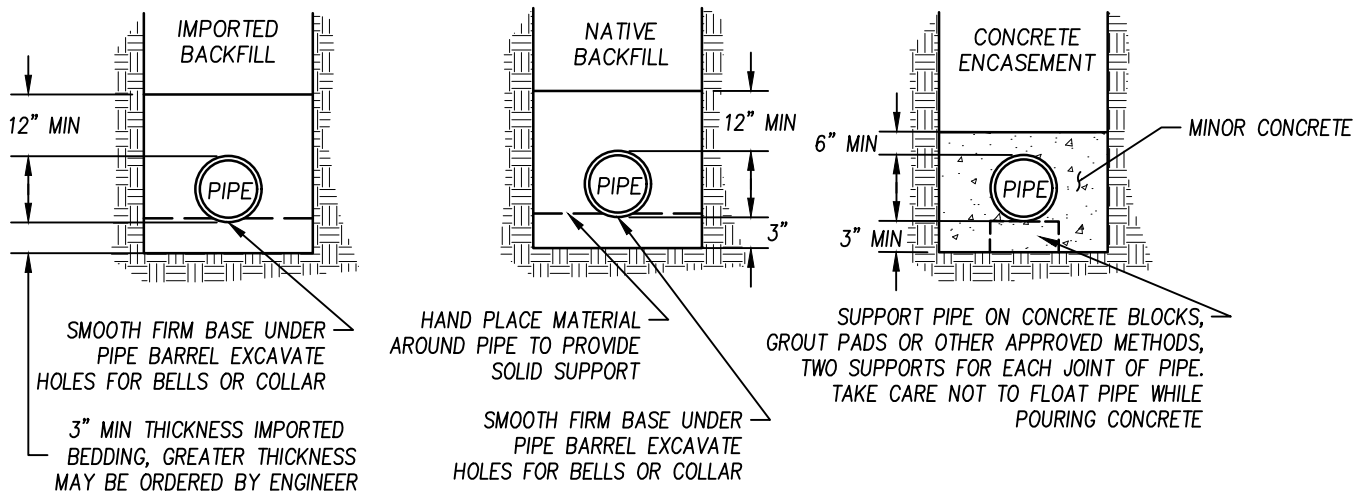
1. FOR TRENCH WIDTH SEE DR-10 FOR STORM DRAIN, W-15 & W-16 FOR WATER PIPE, AND SS-05 FOR SANITARY SEWER PIPE, OF THESE CONSTRUCTION STANDARDS.
2. EXISTING PAVEMENT SHALL BE NEATLY CUT TO A STRAIGHT VERTICAL LINE.
3. AGGREGATE BASE SHALL RECEIVE A "PRIME COAT" OF SC-250 LIQUID ASPHALT OR APPROVED EQUAL. ALL ASPHALT TRENCH PATCHES SHALL RECEIVE A FOG SEAL CONFORMING TO "ASPHALTIC EMULSIONS" OF CALTRANS STANDARD SPECIFICATIONS.

NO.	REVISION DATE	BY	<p style="text-align: center;">TRENCH AND PAVING BACKFILL AND EMBEDMENT</p>	<p>APPROVED BY: DAVID HARDEN</p> <p><i>[Signature]</i> CITY ENGINEER RCE 84216</p>	
DRAWN BY: J. GINNEVER			<p style="text-align: center;">CITY OF GRIDLEY DEPARTMENT OF PUBLIC WORKS</p>	<p style="text-align: center;">ST-20</p>	
CHECKED BY: D. HARDEN					
SCALE: NONE					
DATE: 2/2/26					

BACKFILL DETAILS



BEDDING DETAILS



NOTES:

1. IMPORT BEDDING MATERIAL SHALL BE CLEAN SAND, OR 3/4" MAXIMUM GRAVEL UNIFORMLY GRADED WITH A MINIMUM SAND EQUIVALENT OF 25, OR CLASS 2 AGGREGATE BASE
2. IMPORTED BACKFILL SHALL BE CLEAN SAND OR STREAM GRAVEL WHICH IS REASONABLY WELL GRADED FROM COARSE TO FINE WITH A MAXIMUM SIZE OF 1 1/2" AND NOT LESS THAN 10% PASSING A NO.4 MESH SCREEN AND A MINIMUM SAND EQUIVALENT OF 25, OR CLASS 2 AGGREGATE BASE
3. ALL BEDDING AND BACKFILL SHALL BE COMPACTED TO 95% RELATIVE COMPACTION
4. NATIVE BEDDING AND BACKFILL SHALL HAVE A SAND EQUIVALENT OF 25 OR GREATER AND SHALL BE 3/4" MAXIMUM SIZE
5. THE TRENCH WIDTH SHALL BE A MINIMUM OF THE PIPE OD + 12" AND A MAXIMUM OF THE PIPE OD + 24"

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TRENCH BEDDING AND BACKFILL

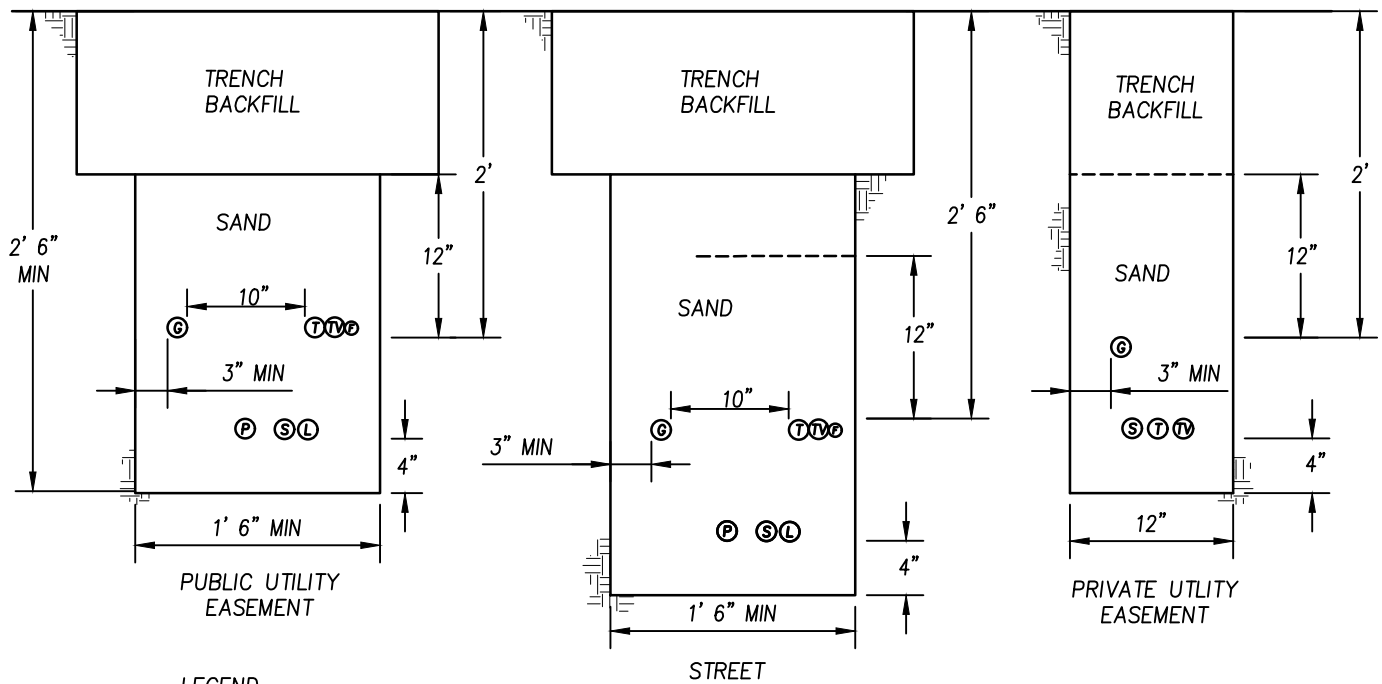
CITY OF GRIDLEY
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CITY ENGINEER
RCE 84216



ST-21



LEGEND

- | | |
|------------------------|------------------|
| Ⓒ – GAS (PG&E) | Ⓓ – TELEVISION |
| ⒫ – PRIMARY ELECTRIC | Ⓔ – FIRE ALARM |
| Ⓖ – SECONDARY ELECTRIC | Ⓛ – STREET LIGHT |
| Ⓣ – TELEPHONE | |

NOTES:

- BACKFILL IN PARKWAY OR STREET INSTALLATIONS SHALL BE CLASS "A" OR "B" AS REQUIRED BY THE PUBLIC WORKS DEPARTMENT.
- ALL ELECTRIC CONDUCTORS PLACED UNDERGROUND SHALL BE INSTALLED IN APPROVED ELECTRICAL CONDUIT:
 - PLASTIC CONDUIT SHALL BE NEMA TC-6 PVC (APPROVED FOR DIRECT BURIAL INSTALLATION) (P&C)
 - ALL CONDUITS INSTALLED SHALL INCLUDE PULL STRINGS
- WHERE PRACTICAL, LOCATE GAS AND ELECTRIC SERVICES AT ALTERNATE LOT LINES TO MINIMIZE CONFLICTS.
- DEPTHS AND SEPARATION SHOWN ARE DESIRABLE MINIMUMS. VARIANCES MAY BE REQUIRED.
- GAS FACILITIES MUST ADHERE TO G0112-F
 - WHERE 10" SEPARATION CANNOT BE OBTAINED IN AN 18" WIDE TRENCH SEPARATION MAY BE REDUCED TO NOT LESS THAN 6" INSTEAD OF INCREASING THE TRENCH WIDTH.
 - WHERE PRIMARY AND GAS ARE NOT INVOLVED SECONDARY MAY OCCUPY POSITION NORMALLY OCCUPIED BY GAS.
- AT NO TIME WILL SANITARY SEWER LINES BE ALLOWED IN TRENCH OCCUPIED BY PRIMARY OR SECONDARY.
- EACH UTILITY COMPANY MAY HAVE ONE OR MORE CONDUITS OR CONDUCTORS IN A TRENCH.
- DEPTHS VARY ACCORDING TO CONDUIT SIZES AND LATERAL INTERFERENCE.
- ALL CONDUCTORS OR CONDUITS SHALL HAVE 3" MINIMUM CLEARANCE FROM TRENCH SIDEWALLS.

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JOINT TRENCH CONFIGURATIONS FOR UTILITY COMPANIES

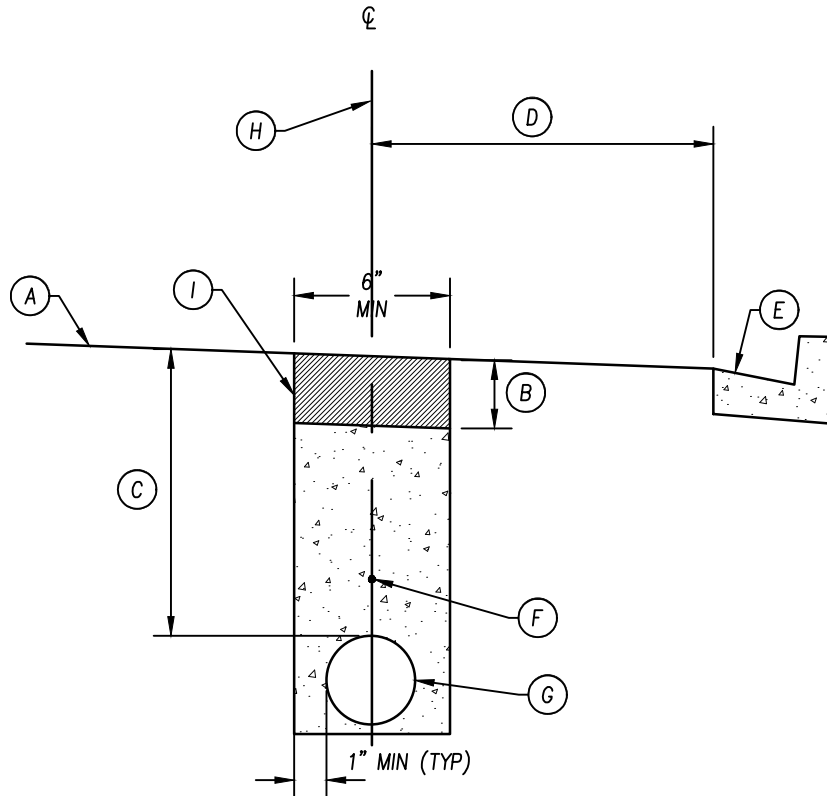
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ST-22



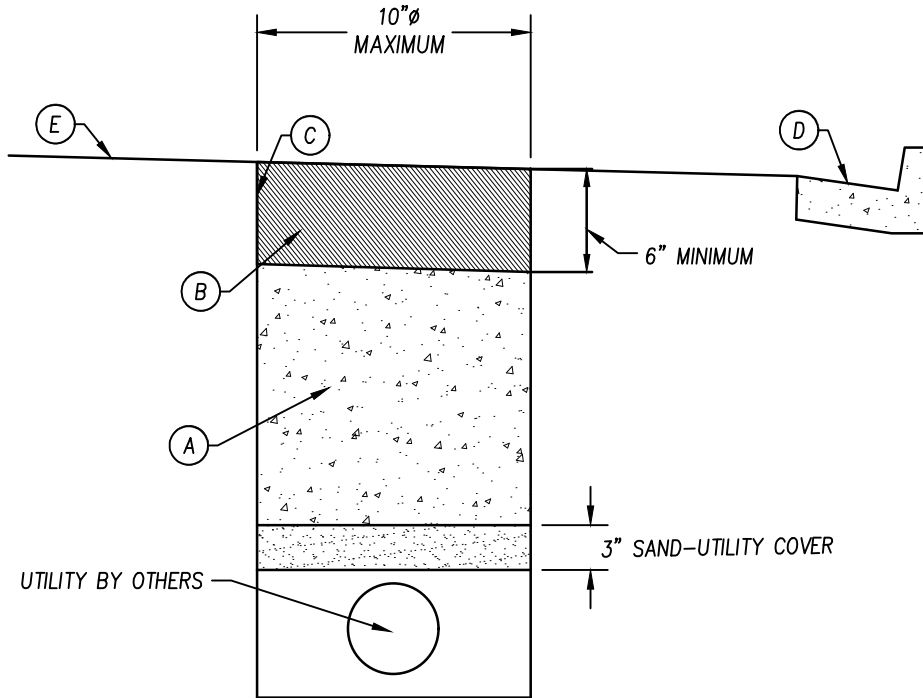
LEGEND:

- A. SURFACE OF EXISTING STREET SECTION
- B. 3" THICK ASPHALT CONCRETE HMA TYPE A 3/4" MIX IN ACCORDANCE WITH CALTRANS STANDARD SPECIFICATIONS (93% RELATIVE COMPACTION)
- C. PIPE COVER PER UTILITY CO.
- D. DISTANCE VARIES. IF LESS THAN 3', THEN CONTRACTOR IS REQUIRED TO EDGE GRIND FROM LIP OF GUTTER (2" DEEP) TO INSIDE LIMIT LINE OF TRENCH
- E. EXISTING CURB AND GUTTER
- F. "MINOR CONCRETE" CONFORMING TO THE PROVISIONS IN CALTRANS STANDARD SPECIFICATIONS, WITH FINE AGGREGATE (PEA GRAVEL MIX). CONCRETE SHALL BE FLOW-ABLE AT DISCRETION OF CITY ENGINEER.
- G. CONDUIT AS SPECIFIED
- H. CENTER LINE OF LANE STRIPE
- I. PLACE BINDER (TACK COAT) ON ALL SURFACES PRIOR TO PAVING PER SECTION 39 OF CALTRANS STANDARD SPECIFICATIONS

NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR REPLACEMENT OF STRIPING AND LEGENDS (AFFECTED BY TRENCHING) WITH THERMOPLASTIC MATERIAL.
2. ALL EXCAVATED AREAS IN THE PAVEMENT SHALL BE BACKFILLED, EXCEPT FOR THE TOP 3" BY THE END OF EACH WORK DAY. THE TOP 3" SHALL BE PLACED WITHIN 3 WORKING DAYS AFTER TRENCHING. DELINEATORS SHALL BE PLACED ON TEN FOOT CENTERS, AND WITHIN 1' OF EACH SIDE OF DRIVEWAYS IN THE INTERIM. REFER TO REQUIRED TRAFFIC CONTROL PLANS FOR TRANSITION LOCATIONS.

NO.	REVISION DATE	BY	<div style="text-align: center;"> <h2>ROCK WHEEL TRENCH</h2> <h3>CITY OF GRIDLEY</h3> <h4>DEPARTMENT OF PUBLIC WORKS</h4> </div>		APPROVED BY:	<div style="text-align: center;">  <h2>ST-23</h2> </div>
					DAVID HARDEN	
					 CITY ENGINEER RCE 84216	
DRAWN BY: J. GINNEVER						
CHECKED BY: D. HARDEN						
SCALE: NONE						
DATE: 2/2/26						





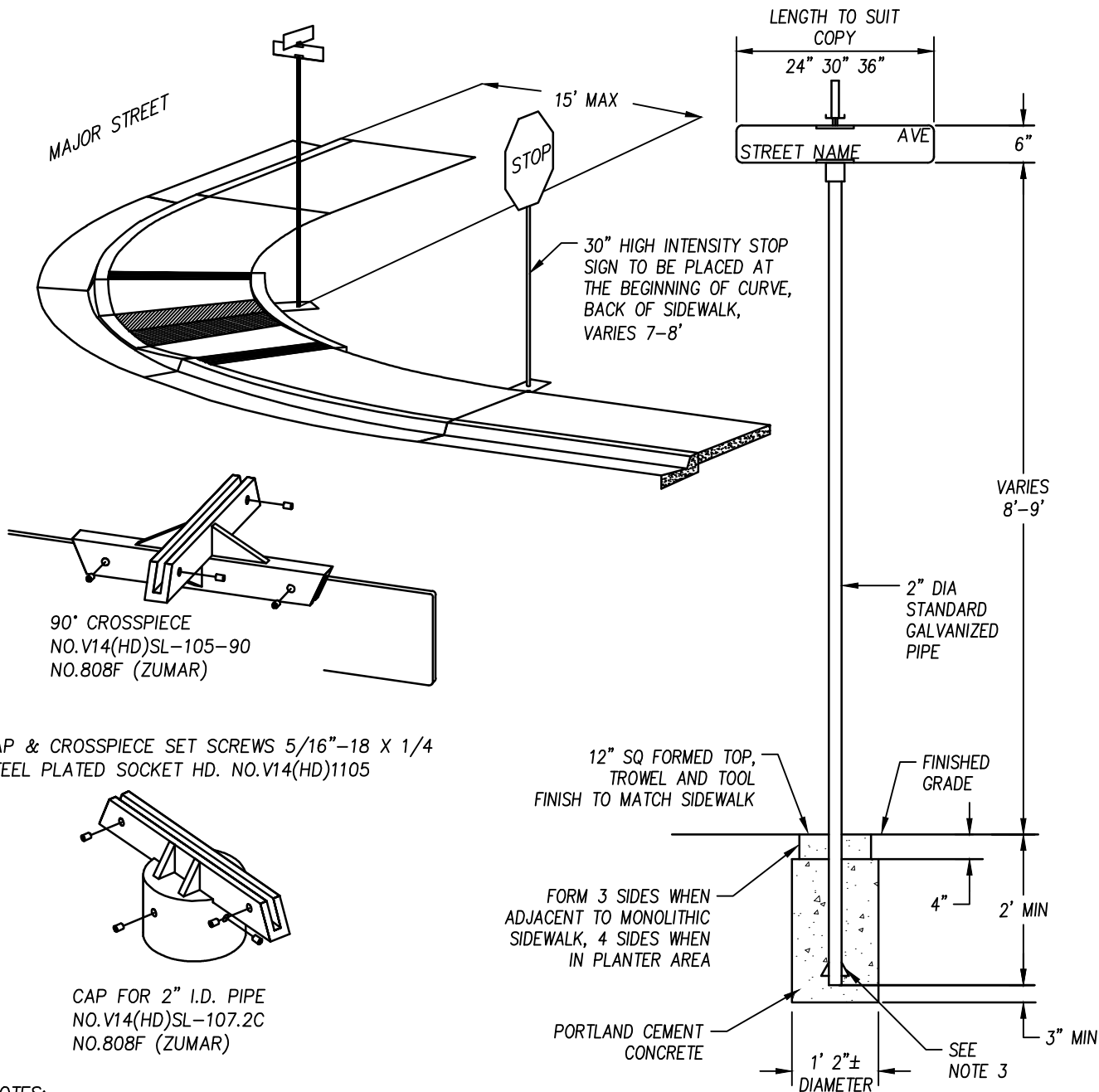
LEGEND:

- A. "MINOR CONCRETE" CONFORMING TO THE PROVISIONS IN CALTRANS STANDARD SPECIFICATIONS "CONCRETE", WITH FINE AGGREGATE (PEA GRAVEL MIX)
- B. 1/2" ASPHALT CONCRETE HMA TYPE A, 3/4" MIX (93% RELATIVE COMPACTION).
- C. PLACE BINDER (TACK COAT) ON ALL SURFACES PRIOR TO PAVING PER SECTION 39 OF CALTRANS STANDARD SPECIFICATIONS
- D. EXISTING CURB AND GUTTER
- E. SURFACE EXISTING STREET SECTION (PAVEMENT)

NOTES:

1. ASPHALT CONCRETE ROADWAY SURFACE POTHOLE SHALL BE SAW CUT OR JACKHAMMERED IN A UNIFORM FASHION. FOR PORTLAND CEMENT CONCRETE ROADWAYS, BRING CONCRETE BACKFILL TO SURFACE, AND FINISH WITH MEDIUM BROOM FINISH.
2. MAXIMUM POTHOLE SIZE SHALL BE 10" DIAMETER. SHOULD POTHOLE SIDEWALL CAVE-IN OCCUR DURING JET-VAC EXCAVATION ADDITIONAL EXCAVATION MAY BE REQUIRED (AT THE DISCRETION OF THE CITY ENGINEER).
3. REPLACE ALL PAVEMENT MARKINGS AND THERMOPLASTIC LEGENDS THAT WERE DAMAGED.
4. POTHOLE EXCAVATION MUST BE TEMPORARILY BACKFILLED SAME DAY AS EXCAVATION AND PERMANENTLY WITHIN SEVEN CALENDAR DAYS.
5. TEMPORARY STEEL COVERS ARE REQUIRED WHEN POTHOLING IS LOCATED WITHIN THE TRAVEL LANES, WHEN OPEN TO TRAFFIC.

NO.	REVISION DATE	BY	POTHOLE RECONSTRUCTION	APPROVED BY: DAVID HARDEN	
DRAWN BY: J. GINNEVER			CITY OF GRIDLEY DEPARTMENT OF PUBLIC WORKS	 CITY ENGINEER RCE 84216	ST-24
CHECKED BY: D. HARDEN					
SCALE: NONE					
DATE: 2/2/26					



NOTES:

1. SIGN LOCATION TO BE DETERMINED BY THE CITY ENGINEER.
2. NAME PLATES & MOUNTING ASSEMBLIES TO MEET THE REQUIREMENTS OF THE CITY ENGINEER.
3. FLARE THE BOTTOM OF PIPE TO PREVENT TURNING.
4. PLATE COLOR SHALL BE WHITE ON GREEN & SHALL BE REFLECTIVE.
5. REFLECTIVE LETTERS TO BE 4" FOR NAME, AND 2" FOR ST., CT., AVE., ETC.
6. 0.080" ALUMINUM PLATE

NOTE:

USE HEAVY DUTY SLOTTED HIGH TENSION ALUMINUM ALLOY DIE CASTINGS FOR CAPS & CROSS PIECES HAWKINS & HAWKINS, ZUMAR (PART NUMBERS GIVEN) OR CITY APPROVED EQUAL.

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SCALE: NONE		
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STREET SIGNS

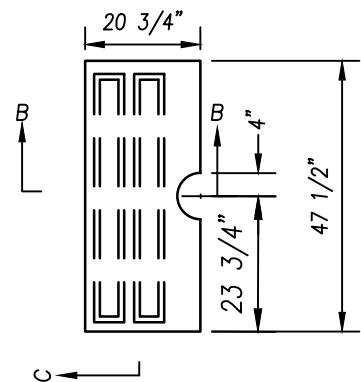
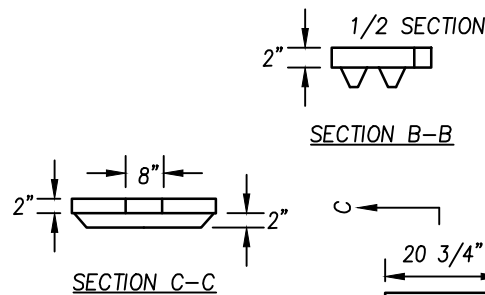
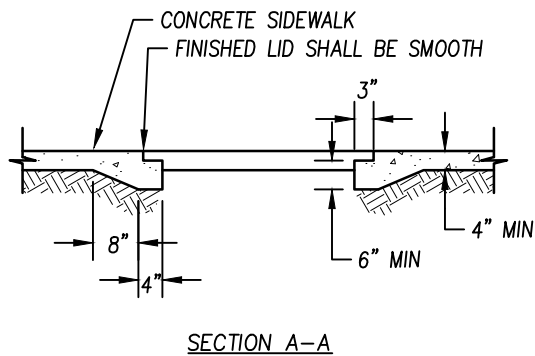
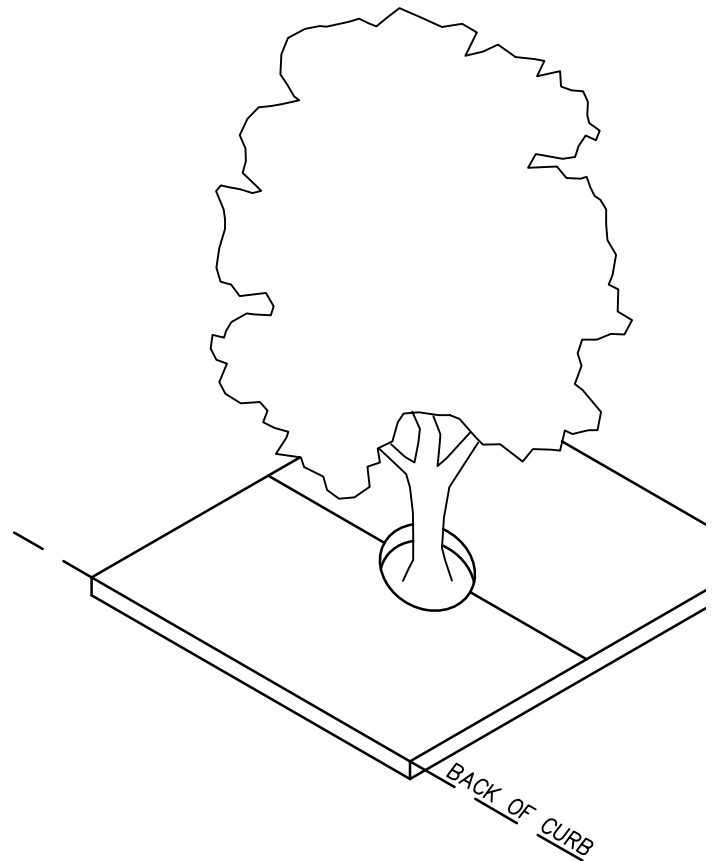
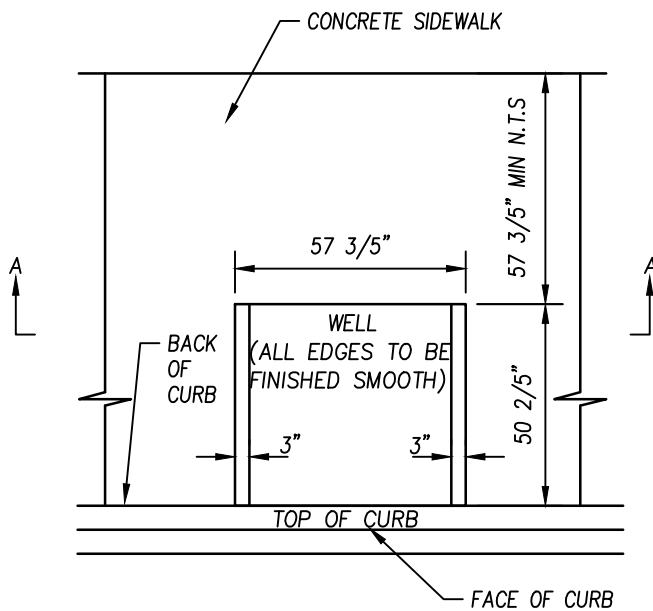
CITY OF GRIDLEY
DEPARTMENT OF PUBLIC WORKS

APPROVED BY:
DAVID HARDEN

D. Harden
CITY ENGINEER
RCE 84216



ST-25



NOTES:

1. ALL DIMENSIONS SHOWN, TO BE HELD EXACTLY TO INSURE PROPER FIT FOR PRECAST COVER.
2. COVER TO BE SIMILAR AND EQUAL TO THOSE MANUFACTURED BY E.E. COOK INC. OR APPROVED EQUAL
3. SPACING AND LOCATION TO BE DESIGNATED BY THE ENGINEER.

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SCALE: NONE		
DATE: 2/2/26		

TREE AND WELL COVER

CITY OF GRIDLEY
DEPARTMENT OF PUBLIC WORKS

APPROVED BY:
DAVID HARDEN

D. Harden
CITY ENGINEER
RCE 84216



ST-26